

DGM-64-25 COW September 9, 2025
William Coutts presentation

100 Plains Rd

Review of the Recommendation Report
for Official Plan and Zoning By-law Amendment
for 100 Plains Road East

Human



Overview

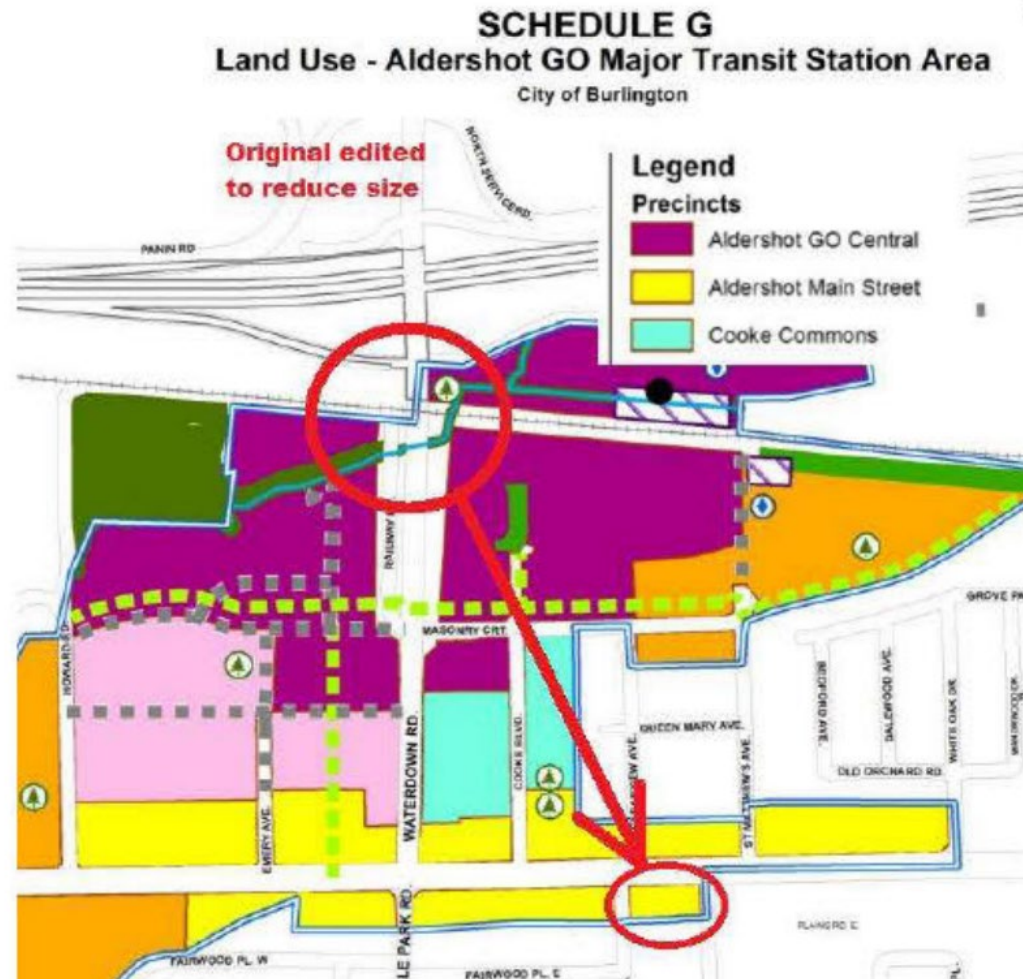
- Transition of Built Form to Adjacent Areas
 - Requirements
 - Sufficiency of the evidence of compliance provided
- Egress and ingress on residential streets
 - Requirements
 - Sufficiency of the evidence of compliance provided
 - Pedestrians

Provincial Policy Statement (PPS)

PPS 2.4.1.3

- b) identify the appropriate type and scale of development in *strategic growth areas* and the transition of built form to adjacent areas;

Context: Subject Lands in the MTSA



Context: Proposal is the Tallest in the Vicinity

intended to host mix use buildings with mid-rise built forms with gradually taller buildings towards the Aldershot GO station. The intent of regulating maximum heights for the lower _____

Number	Address	Storeys (Rank)	FAR (Rank)	R+J/ ha (Rank)	Additional Information
Proposal	84, 92 and 104 Plains Road East and 990 Glenwood Avenue	11 (1)	4.88 (1)	1,800 (1) ⁶	The tallest building with the largest intensity and density of any project in the immediate vicinity.

Taller than any adjacent proposal.

Taller than any proposal towards the GO Station.



Requirement to Transition between MTSA and Adjacent Existing Neighborhood

Staff PPS Analysis

In accordance with Section 2.4.1 3., planning authorities should identify the appropriate type and scale of development in Strategic Growth Areas and the transition of built form to adjacent areas as well as permit development and intensification within these to support the achievement of complete communities and a compact built form. The proposed development has considered transition to adjacent areas based on the applicable Official Plan design policies and associated design guidelines while providing for residential intensification as later discussed in this analysis.

City of Burlington Official Plan (1997)

The Official Plan⁴ includes numerous references to compatibility with existing neighborhoods:

- Compatibility in all land use decisions;
- Design shall consider the surrounding neighborhood;
- Infill will preserve and enhance the quality of life;
- Intensification and infill within existing neighborhoods must be designed to be compatible and sympathetic to existing neighbourhood character;
- Intensification must be evaluated against the need for compatibility with the existing residential neighborhood;
- Redevelopment must be integrated within existing urban fabric;
- Discourage traffic through existing residential neighborhoods;
- Trucks (garbage trucks) and heavy traffic should not go through existing residential neighborhoods if avoidable.

What evidence is used to prove compliance?

The Angular Plane Study

Part III, Section 2.5.2 a) (v): compatibility is achieved with the existing neighbourhood character in terms of scale, massing, height, siting, setbacks, coverage, parking and amenity area so that a transition between existing and proposed buildings is provided.

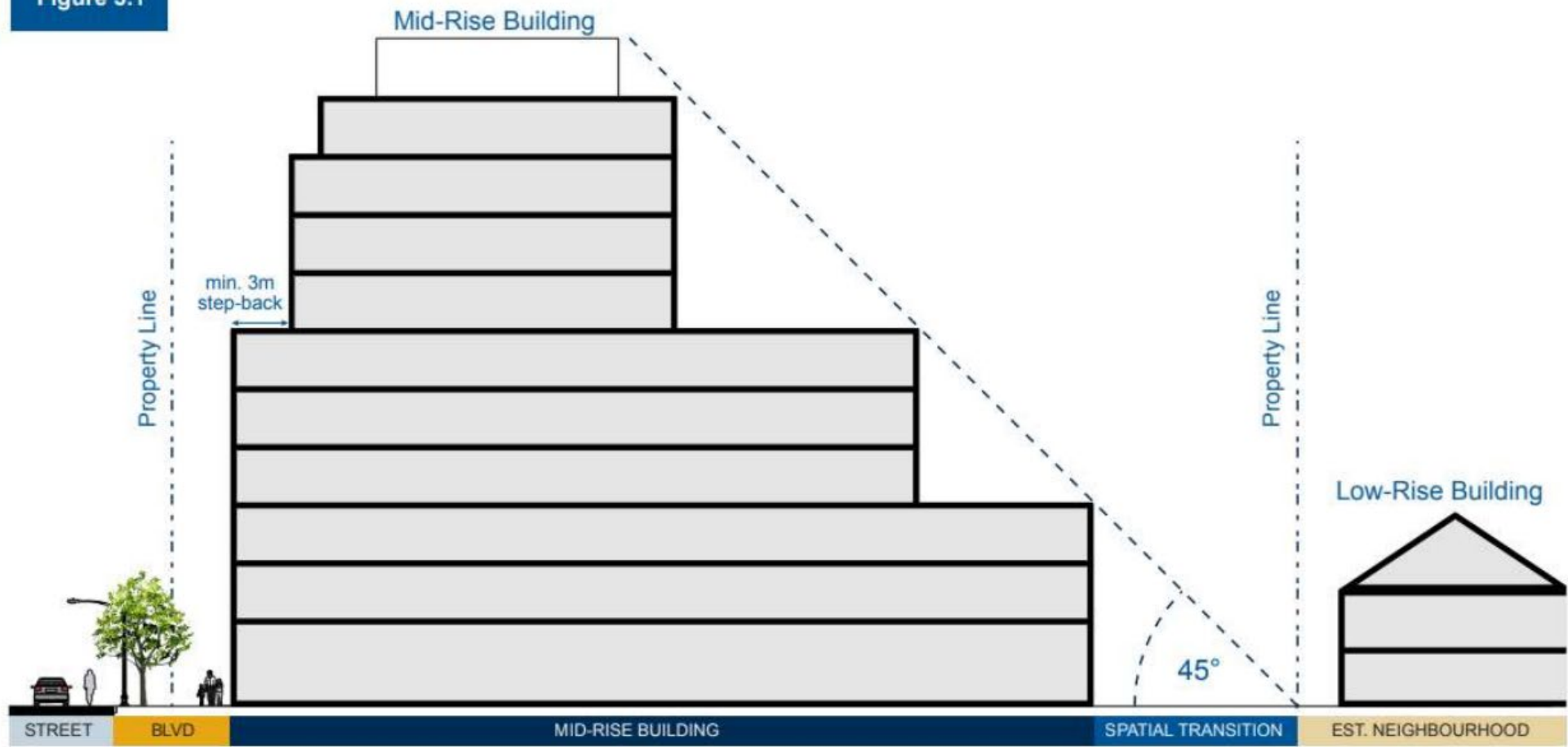
building maintains compatibility with the existing neighbourhood character in terms of scale, massing, height, siting, setbacks, coverage as later discussed under the analysis of the Zoning By-law and applicable Urban Design Guidelines including the Mid-Rise Building Guidelines, Pedestrian Level Wind Study Guidelines and Shadow Study Guidelines.

distance between neighbouring buildings with windows is to protect for privacy within residential units and discourage overlook. Similarly, an angular plane study was also submitted along with the application materials in accordance with Section 3.1 3) of the Design Guidelines for Mixed-Use and Residential Mid-Rise Buildings. As later discussed

The Angular Plane

- 3. Where the building is on a site that is transitioning to a low-rise residential neighbourhood area (including properties designated Residential – Low Density and – Medium Density) a 45-degree angular plane should be applied from the shared property line. The building form should fit entirely within this angular plane and utilize setbacks and step-backs to ensure any impacts related to the change in height, overlook, and shadowing, and pedestrian level wind impacts are mitigated.*

Figure 3.1



The *Revised* Angular Plane Diagram

The proposed development also abuts a low-rise residential neighbourhood uses to the rear property line. As previously mentioned, the initial application submission the applicant provided a 45-degree Angular Plane diagram measured from the top of the fence along the rear property line. Subsequently, staff requested that the applicant provide a revised Angular Plane diagram measured from fixed grade from the rear property line. Staff were able to confirm that the proposed building is generally found within the 45-degree Angular Plane. Staff is of the opinion that although sections of the building encroach into the 45-degree angular plane, impacts of potential overlook into the neighbouring properties to the rear are mitigated by way of the proposed 2.8m landscape buffer and proposed 3m high fence which have been incorporated into the Zoning By-law.

- [Phase One Environmental Site Assessment for property 92 Plains Road East](#) prepared by Soil-Mat Engineers & Consultants Ltd. dated April 30, 2025.
- [Phase Two Environmental Site Assessment for property 92 Plains Road East](#) prepared by Rubidium Environmental Inc. dated November 21, 2017.
- [Phase One Environmental Site Assessment Letter of Reliance](#) prepared by MTE Consultants Inc. dated May 9, 2025.
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- [Notification of Intent to Use Non-Potable Groundwater Site Condition Standards](#) prepared by Halton Region dated May 6, 2025.
- [Waste Management Plan](#) prepared by Kozlowski Architect dated May 21, 2025.
- [Waste Management Report](#) prepared by Kozlowski Architect dated January 27, 2025
- [Reliance Letter for Functional Servicing Report](#) prepared by Trafalgar Engineering Ltd. dated May 12, 2025.
- [Urban Design Brief](#) prepared by MHBC Planning Ltd. dated May 2025.
- [Electrical Plans](#) prepared by EJS Engineering dated May 6, 2025.
- [Revised Land Use Compatibility Study](#) prepared by RWDI Air Inc. dated July 31, 2025.
- [Tenant Relocation and Assistance Plan](#) prepared by Miura Development Group Inc. dated May 23, 2025.
- [Tree 305 Implications](#) prepared by Miura Development Group dated May 9, 2025.
- [Revised Conceptual Site Plan and Architectural Plans](#) prepared by Kozlowski Architect dated July 28, 2025.
- [Revised Angular Plane](#) prepared by Kozlowski Architect dated August 14, 2025

The supporting documents have been uploaded on the City's website for the subject application which can be found on the following link www.burlington.ca/100plainsroad

Supporting Documents

Description

Air Quality Assessment

[Download Air Quality Assessment - 11-22-2024]

Application Form

[Download Application Form - 05-16-2025]

Arborist Report

[Download Arborist Report - 05-09-2025]

City of Burlington and Halton Region Letter of Reliance

[Download City of Burlington and Halton Region Letter of Reliance - 05-12-2025]

Conceptual Landscape Plan

[Download Conceptual Landscape Plan - 05-09-2025]

Conceptual Site Plan and Architectural Plans

[Download Conceptual Site Plan and Architectural Plans - 05-09-2025]

Electrical Plans

[Download Electrical Plans - 05-06-2025]

Elevation Renderings

[Download Elevation Renderings - 06-11-2025]

ESA Validity Letter

[Download ESA Validity Letter - 08-08-2019]

ESSQ

[Download ESSQ - 01-27-2025]

FSR and SWM Report

[Download FSR and SWM Report - 05-12-2025]

Geotechnical Study Hydrogeological Assessment

[Download Geotechnical Study Hydrogeological Assessment - 06-06-2025]

Grading Plan

[Download Grading Plan - 05-12-2025]

Housing Impact Statement

[Download Housing Impact Statement - 01-05-2025]

Land Use Compatibility Study

[Download Land Use Compatibility Study - 05-23-2025]

Letter of Reliance

[Download Letter of Reliance - 05-09-2025]

Noise and Vibration Study

[Download Noise-and-Vibration-Study-05-08-2025]

Non Potable Notification

[Download Non Potable Notification - 05-06-2025]

Pedestrian Level Wind Study

[Download Pedestrian Level Wind Study - 05-08-2025]

Phase One ESA

[Download Phase One ESA]

Phase One ESA - 92 Plains Rd E

[Download Phase One ESA - 92 Plains Rd E]

Phase Two ESA

[Download Phase Two ESA]

Phase Two ESA - 92 Plains Rd E

[Download Phase Two ESA - 92 Plains Rd E]

Planning Justification Report

[Download Planning Justification Report - 01-05-2025]

Planning Justification Report

[Download Planning Justification Report - 01-05-2025]

Record of Site Condition

[Download Record of Site Condition - 02-21-2018]

Revised Land Use Compatibility Study

[Download REVISED-Land-Use-Compatibility-Study---7-31-2025]

Servicing Plan

[Download Servicing Plan - 05-12-2025]

Shadow Study

[Download Shadow Study - 11-18-2024]

Survey

[Download Survey - 01-18-2024]

TIS TDM Paving Justification

[Download TIS TDM Paving Justification - 05-23-2025]

Tree Inventory and Preservation Plan

[Download Tree Inventory and Preservation Plan - 05-09-2025]

Urban Design Brief

[Download Urban Design Brief - 01-05-2025]

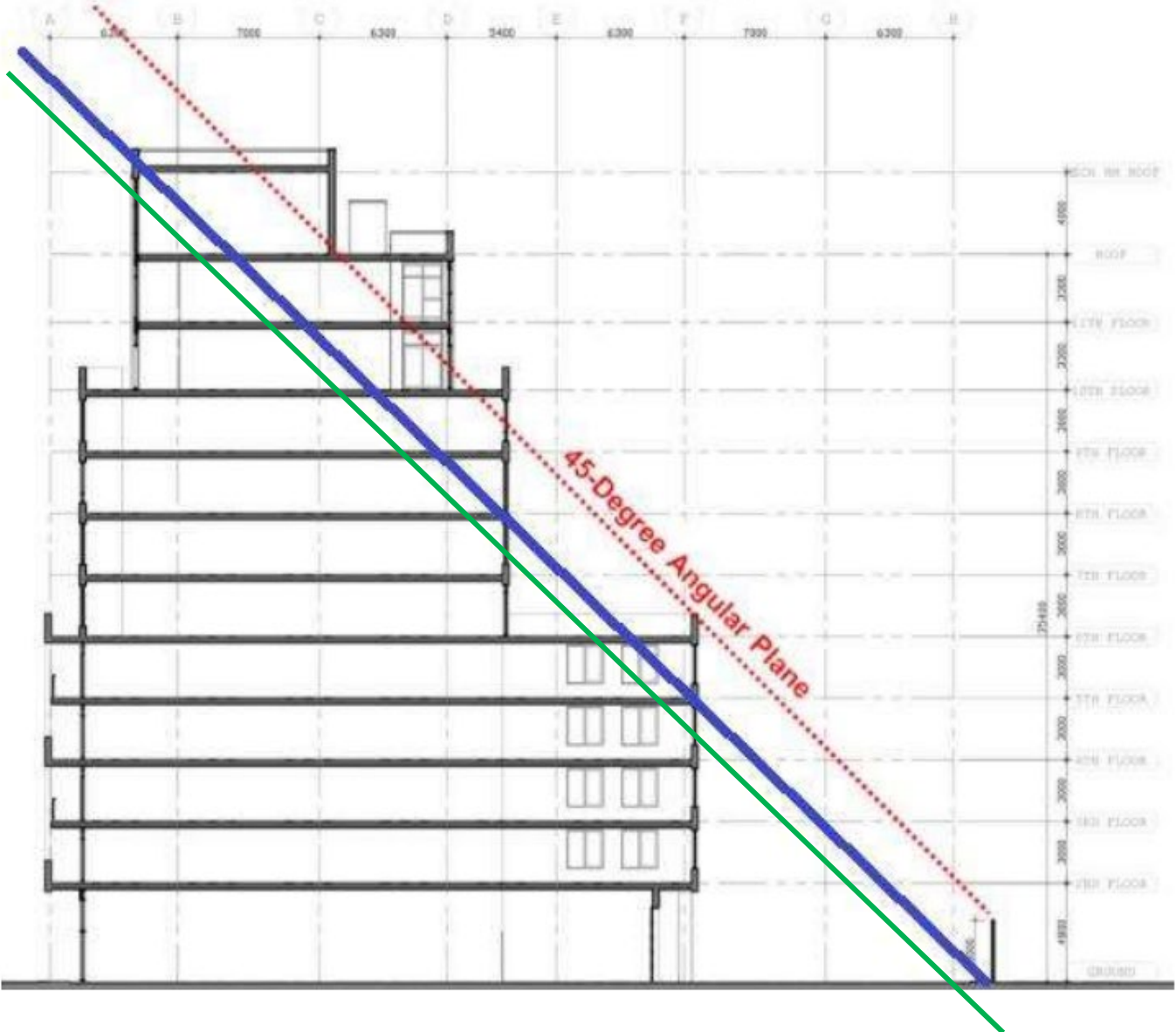
Waste Management Plan

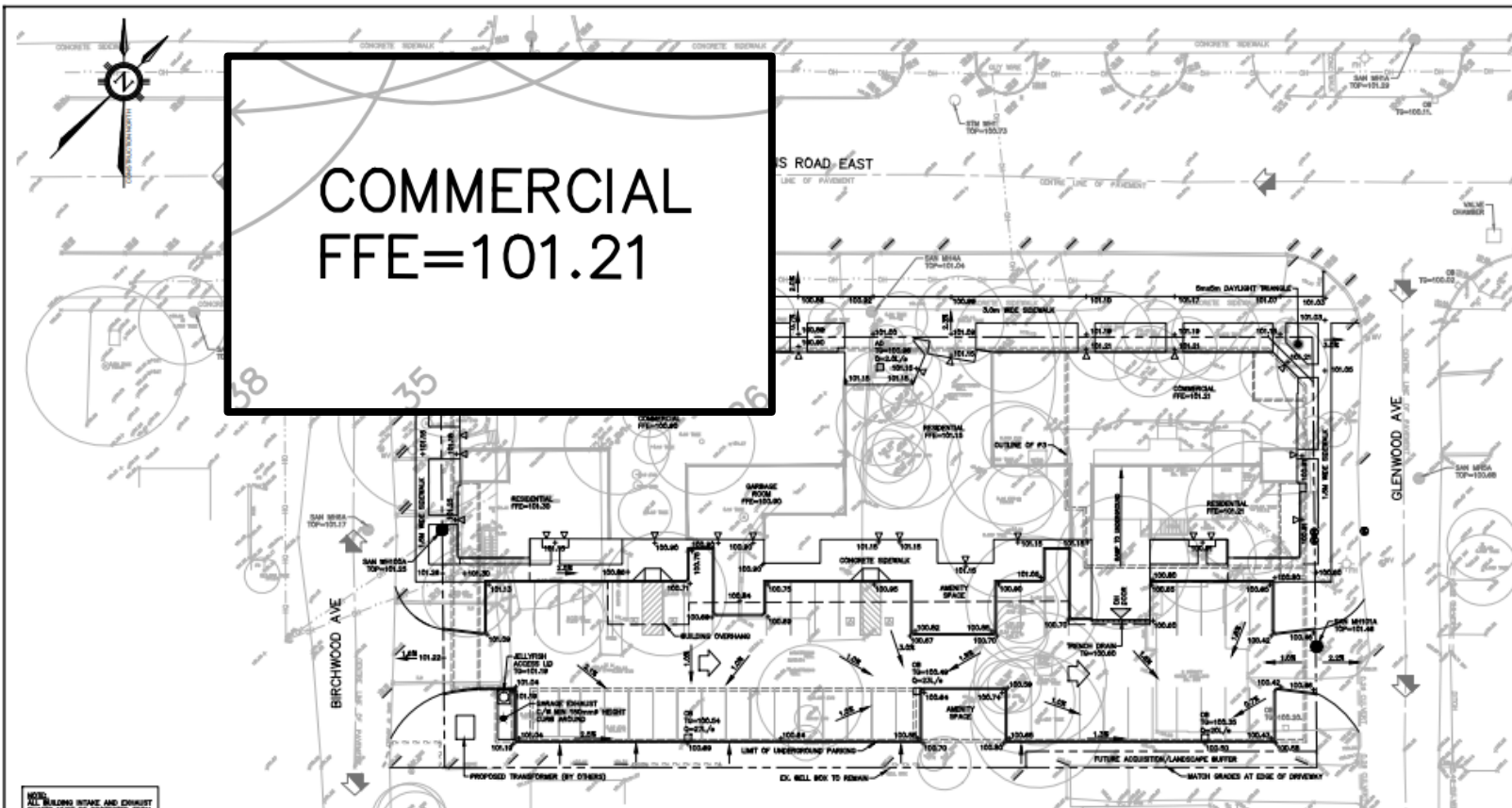
[Download Waste Management Plan - 05-21-2025]

Waste Management Report

[Download Waste Management Report - 01-27-2025]

At Underground Garage Entrance





COMMERCIAL
FFE=101.21

NOTE:
ALL BUILDING INTAKE AND EXHAUST DUCTS MUST BE PROTECTED FROM OVERLAND FLOW BY MINIMUM 0.15M ABOVE ADJACENT GRADE.

NOTE:
ALL WELLS AND DRAINS LOCATED OVER THE UNDERGROUND PARKING GARAGE OR WITHIN 1.0M OF THE BUILDING SHALL BE DEEMED EXISTING BASE AND NEEDS BY THE MECHANICAL CONTRACTOR AND BE CAPABLE OF ACCEPTING THE PROPOSED OVERLAND FLOW WITH SOIL BLOCKAGE.

PRIOR TO COMMENCING ANY WORK WITH THE MUNICIPAL RIGHT-OF-WAY, THE CONTRACTOR, DEVELOPER OR CONSULTANT WILL OBTAIN ALL NECESSARY ROAD OCCUPANCY PERMITS FROM THE TOWN'S RIGHT-OF-WAY MANAGEMENT SECTION.

GENERAL NOTES

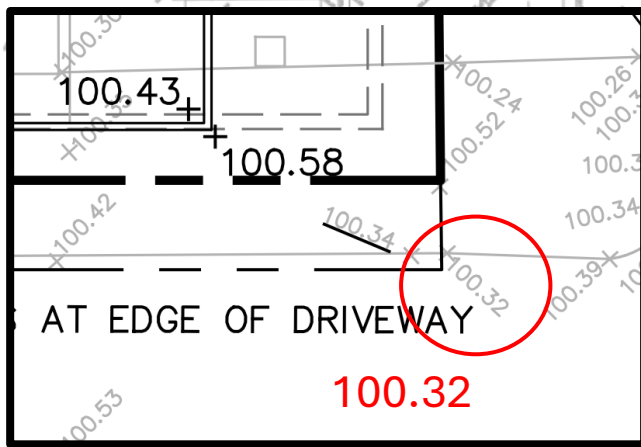
- ALL THE CONSTRUCTION WORK FOR THIS PROJECT SHALL COMPLY WITH THE STANDARD DRAINAGES AND SPECIFICATIONS OF THE CITY OF BURLINGTON AND THE ONTARIO PROVINCIAL STANDARDS AND SPECIFICATIONS.
- ALL SURFACE DRAINAGE SHALL BE COLLECTED AND DISCHARGED AT A LOCATION TO BE APPROVED PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. DRAINAGE OF ADJUTING PROPERTIES SHALL NOT BE ADVERSELY AFFECTED.
- PROPOSED DRAINAGES ALONG SITE PROPERTY LINES MUST MATCH EXISTING ELEVATIONS.
- A BELT FENCE MUST BE PLACED AROUND THE FORMER OF THE SITE.
- AT ALL ENTRANCES TO THE SITE, THE ROAD CURB AND SIDEWALK WILL BE CONTIGUOUS THROUGH THE DRIVEWAY. THE DRIVEWAY GRADE WILL BE COMPATIBLE WITH THE EXISTING SIDEWALK AND A CURB DEPRESSION WILL BE PROVIDED FOR AT EACH ENTRANCE.
- SEWERAGE TO BE REMOVED AND REPLACED AS PER CITY STD. 5-151.
- THE PORTION OF THE DRIVEWAY WITHIN THE MUNICIPAL BOUNDARY MUST BE PAVED WITH 40mm 14.3 and 10mm H.S.B. BASE TO BE 150mm GRANULAR A (OR 130mm OF 20mm CRUSHER RUN LIMESTONE) AND 300mm GRANULAR B (OR 225mm OF 20mm CRUSHER RUN LIMESTONE) COMPACTED TO 100% STANDARD PROCTOR DENSITY.
- A UTILITY CLEARANCE HEIGHT OF 1.2 METERS BETWEEN THE PROPOSED DRIVEWAY ENTRANCE CURB RETURN AND ALL ABOVE GROUND UTILITIES MUST BE MAINTAINED.
- ROAD OCCUPANCY / ACCESS PERMIT MUST BE OBTAINED 48 HOURS PRIOR TO COMMENCING ANY WORKS WITHIN THE MUNICIPAL ROAD ALLOWANCE.
- THE SERVICE CONNECTION TRENCH WITHIN THE TRAVELLED PORTION OF THE ROAD ALLOWANCE SHALL BE BACKFILLED IN ACCORDANCE WITH THE REQUIREMENTS OF THE ROAD OCCUPANCY / ACCESS PERMIT APPLICATION.
- THE WINDRUP CATCHBASIN LEAD DAMPER ALLOWED IS 200mm.
- ALL CATCHBASIN MANHOLES AND MANHOLES WITH INLET CONTROL DEVICES MUST HAVE A MINIMUM 0.3 METRE SWAMP AND TOP AS PER MUNICIPAL STANDARDS.
- RESTORE ROAD AS PER THE CITY OF BURLINGTON ROAD CUT PERMIT.
- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL ARCHITECTURAL, MECHANICAL, AND LANDSCAPE DRAWINGS.

GRADING NOTES

- ALONG ADJOINING PROPERTIES GRADE TO MEET EXISTING OR PROPOSED ELEVATIONS WITH SLOPED SLOPES (MIN. 3% TO 1%) AND/OR RETAINING WALLS AS SPECIFIED.
- UNLESS OTHERWISE NOTED, THE GRADING BETWEEN PROPOSED ELEVATIONS ON SEE LOTS SHALL BE GRADED AS A STRAIGHT LINE.
- ALL FILL PLACED ON LOTS SHALL BE COMPACTED TO A MINIMUM 90% UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER. ALL MATERIAL SHALL BE PLACED IN LAYERS NOT EXCEEDING 300mm LIFTS.
- IF APPLICABLE FOR DELINEATION OF TREE PROTECTION ZONES, BUTTERS, REMOVALS AND PROTECTION SOLEMETS, ETC., REFER TO TREE PROTECTION PLAN (BY LANDSCAPE ARCHITECT).
- LOT GRADING SHALL CONFORM STRICTLY WITH THIS PLAN. ANY CHANGES, UNLESS APPROVED PRIOR TO CONSTRUCTION BY THE CITY, SHALL RESULT IN NON COMPLIANCE BY THE CITY.
- ALL TOPSOIL SHALL BE STRIPPED PRIOR TO GRADING.
- ALL DISTURBED AREAS TO BE RESTORED WITH 300mm TOPSOIL AND SO2.
- DRIVEWAY SLOPE SHALL BE A MINIMUM OF 1.0% AND A MAXIMUM OF 8.0% AS PER CITY OF BURLINGTON STANDARDS, UNLESS OTHERWISE INDICATED.

ADDITIONAL NOTES:

- LOCATION OF ALL EXISTING UTILITIES IN THE FIELD TO BE ESTABLISHED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATING, EXPOSING, SUPPORTING AND PROTECTING ALL UNDERGROUND AND OVERHEAD UTILITIES AND STRUCTURES EXISTING AT THE TIME OF CONSTRUCTION IN THE AREA OF HIS WORK. WEATHER SHOWN ON THE PLAN OR NOT, AND FOR ALL REPAIRS AND CONSEQUENCES RESULTING FROM DAMAGE TO SAME.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE TO GIVE 72 HOURS WRITTEN NOTICE TO UTILITIES PRIOR TO GRADING SUCH UTILITIES, FOR THE PURPOSE OF INSPECTION BY THE OWNED UTILITY. THIS INSPECTION WILL BE FOR THE QUALITY OF THE CONSTRUCTION, WITH THE CONTRACTOR RESPONSIBLE FOR ALL COST ARISING FROM SUCH INSPECTION.



AT EDGE OF DRIVEWAY

100.32



LEGEND

---	PROPERTY BOUNDARY
---	EXISTING ELEVATION
---	PROPOSED FINISHED ELEVATION
---	PROPOSED DRAINAGE DIRECTION
---	EXISTING FIRE HYDRANT
---	EXISTING STORM MANHOLE
---	EXISTING SANITARY MANHOLE
---	EXISTING CATCHBASIN
---	EXISTING DOUBLE CATCHBASIN
---	EXISTING VALVE AND BOX
---	EXISTING STORM SEWER
---	EXISTING SANITARY SEWER
---	EXISTING WATERMAIN
---	PROPOSED OVERHEAD ENTRANCE
---	PROPOSED OVERLAND FLOW DIRECTION
---	EXISTING OVERLAND FLOW DIRECTION
---	PROPOSED CATCHBASIN
---	PROPOSED SANITARY MANHOLE
---	PROPOSED AREA DRAIN
---	PROPOSED STORM MANHOLE
---	PROPOSED VALVE & BOX
---	TREE HEADINGS

NO.	DATE	ISSUED FOR
1	2024/10/01	ISSUED FOR DEVELOPMENT REVIEW
2	2024/10/01	ISSUED FOR DEVELOPMENT REVIEW
3	2024/10/01	ISSUED FOR DEVELOPMENT REVIEW

ELEVATION NOTE:
ELEVATIONS ARE IN METERS, DERIVED FROM OBSERVED REFERENCE POINTS A & B BY REAL TIME NETWORK OBSERVATION, USE ZONE 17, NAD83 (CSRS) (2011).

LOCAL BENCHMARK:
BENCHMARK 44 - IMPERIAL STREET SCHOOL, 481 PLAINS ROAD EAST (HIGHWAY NO. 2) 1.80 KM NORTHEAST OF WATERLOO ROAD TABLET IN THE FRONT OF THE SCHOOL'S CONCRETE FOUNDATION WALL, 1.5M SOUTHWEST OF THE MOST EASTLY OF TWO ENTRANCES, 1.54M BELOW RED ROCK SURFACE NO. 3464. ELEVATION: 98.774 METERS.

THE TOPOGRAPHIC DETAIL SHOWN HEREON WAS ACQUIRED ON JANUARY 17, 2024 BY A.T. HOLDEN LIMITED (DNR NO. 20462).

DESIGNED BY: [Signature]

APPROVED BY: [Signature]



PROJECT TITLE: ONE HUNDRED ON PLAINS

LOCATION: 84-104 PLAINS ROAD EAST & 990 GLENWOOD AVENUE, BURLINGTON, ONTARIO

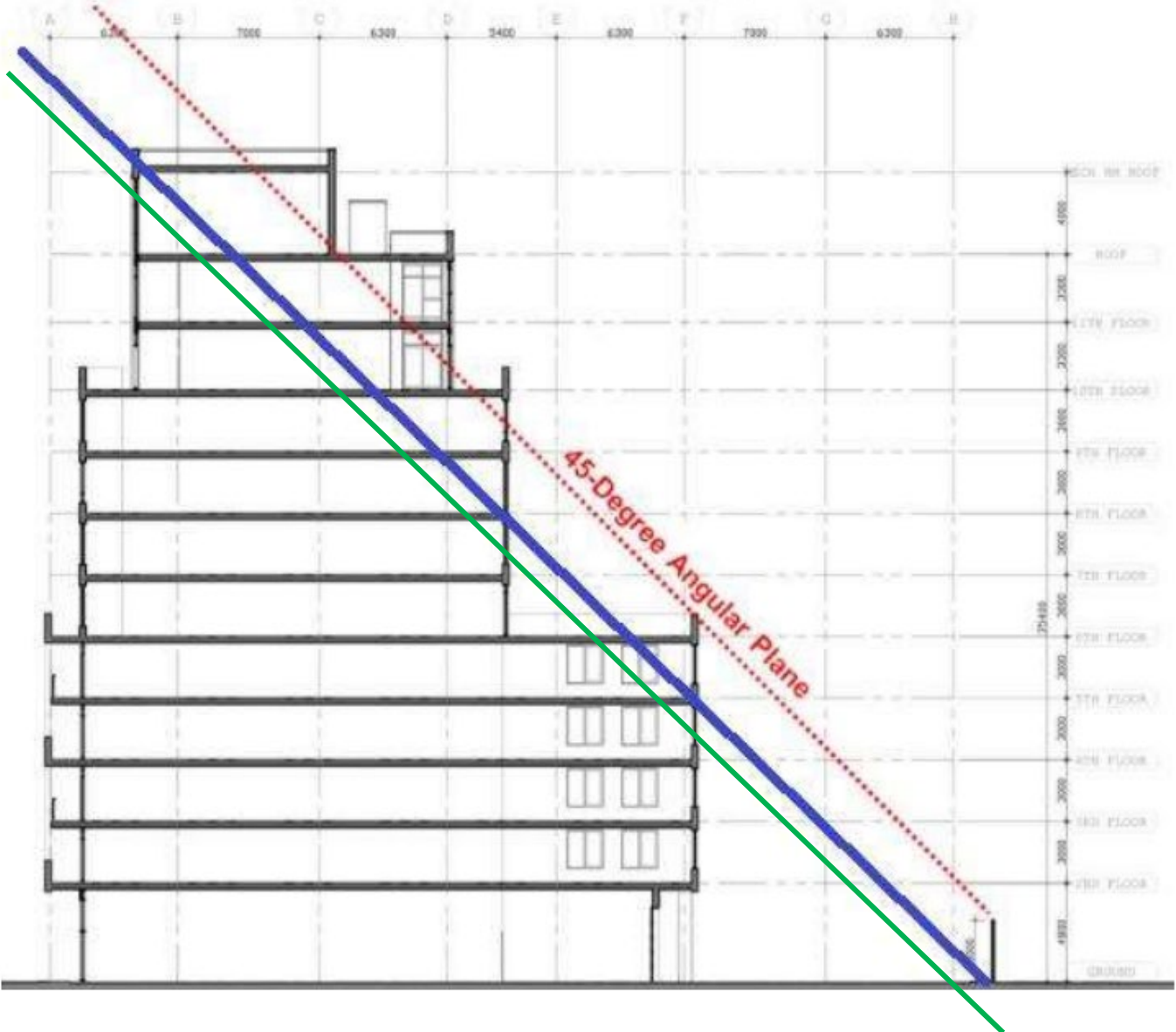
GRADING PLAN

SCALE: 1:200	DRAWN BY: HAD	PROJECT NO.: 1881
DATE: 2024/10/01	CHECKED BY: HAD	PLAN NO.: G1
	SHEET: 1 OF 1	

The balconies are missing

5. *Balconies and other projections should be contained within all angular planes and setbacks and shall not protrude into the public realm (over sidewalks).*

At Underground Garage Entrance



Proposed Mitigation

The proposed development also abuts a low-rise residential neighbourhood uses to the rear property line. As previously mentioned, the initial application submission the applicant provided a 45-degree Angular Plane diagram measured from the top of the fence along the rear property line. Subsequently, staff requested that the applicant provide a revised Angular Plane diagram measured from fixed grade from the rear property line. Staff were able to confirm that the proposed building is generally found within the 45-degree Angular Plane. Staff is of the opinion that although sections of the building encroach into the 45-degree angular plane, impacts of potential overlook into the neighbouring properties to the rear are mitigated by way of the proposed 2.8m landscape buffer and proposed 3m high fence which have been incorporated into the Zoning By-law.



10th Storey



7th Storey



10th Storey



7th Storey



Urban Design Panel Review

Transition to the South

The Panel raised concerns about the proposed buffer provided for the residential low-rise to the south, noting that it is quite narrow.

The Panel expressed concern that the current massing and parking configuration is maximized on site, leaving little room for the residents to the south.

They recommended considering a balance between density and providing adequate space for neighbouring residents.

East and West Step-Backs



Context: Urban Design Panel Review

Transition to the East

The Panel recognized that the cemetery to the east clearly marks the end of the MTSA.

Suggested reducing the building massing as it moves eastward ... stepping down towards the neighborhood, which is unlikely to undergo further transformation.

The Panel emphasized the significance of the building's eastern elevation, noting its prominence as the endpoint of the MTSA and its visibility due to adjacent lower-density development.

Context: Other development in the MTSA

Number	Address	Step-backs, Setbacks, etc.
Proposal	84, 92 and 104 Plains Road East and 990 Glenwood Avenue	Does not meet the 45° angle test. Does not set step-back from east or west boundaries to low-rise residential. Minimal setbacks (e.g., 0.89m from Glenwood).
5	40 - 70 Plains Road East	The Tribunal settlement differentiates the two ends of this complex, restricting the easterly development to 10 stories and requiring step-backs and 5.5 m setbacks from the “key transition point at the south-east of the property to abutting low rise, low density residential development.” Includes concealed vehicular access from Plains Road East.
4	53 - 71 Plains Road East & 1025 Cooke Blvd	The northwest section is situated on Cooke Blvd. and is adjacent to high-rise proposals. The 9-story section of the project is on Plains Rd E and is proposed to be separated from low-rise residential <i>by a park</i> to be installed by the developer.
7	35 Plains Road East	This project is close to Waterdown Road and is not adjacent to any low-rise residential that is not already the subject of redevelopment.
9	1085 Clearview Avenue	An MTSA boundary property adjacent to low-rise residential. This property is limited to 6 stories, has significant step-backs and setbacks on multiple sides, and has a “townhouse façade” on one side to help with the transition between the MTSA and the adjacent low-rise low-density residential street.

Step-backs claimed in the Report

Report Analysis:

towards the Aldershot GO station. The intent of regulating maximum heights for the lower portion of mid-rise buildings is to achieve human scale and pedestrian feel along the streetscape. The proposed lower building portion includes step-backs along the west, east and south elevations at the 6th, 8th and 10th storeys, at approximately 16.8m, 22.8m and 28.8m in height measured from fixed grade, to provide for building design articulation.

- 5. Where a streetwall is not established, the streetwall for new mid-rise buildings should be limited to a height of 80% of the street width (up to a maximum of 6-storeys) with additional storeys stepping-back a minimum of 3 metres above the streetwall to maintain a humanscale and minimize shadowing. On streets with a*

Steps-back 0.8m



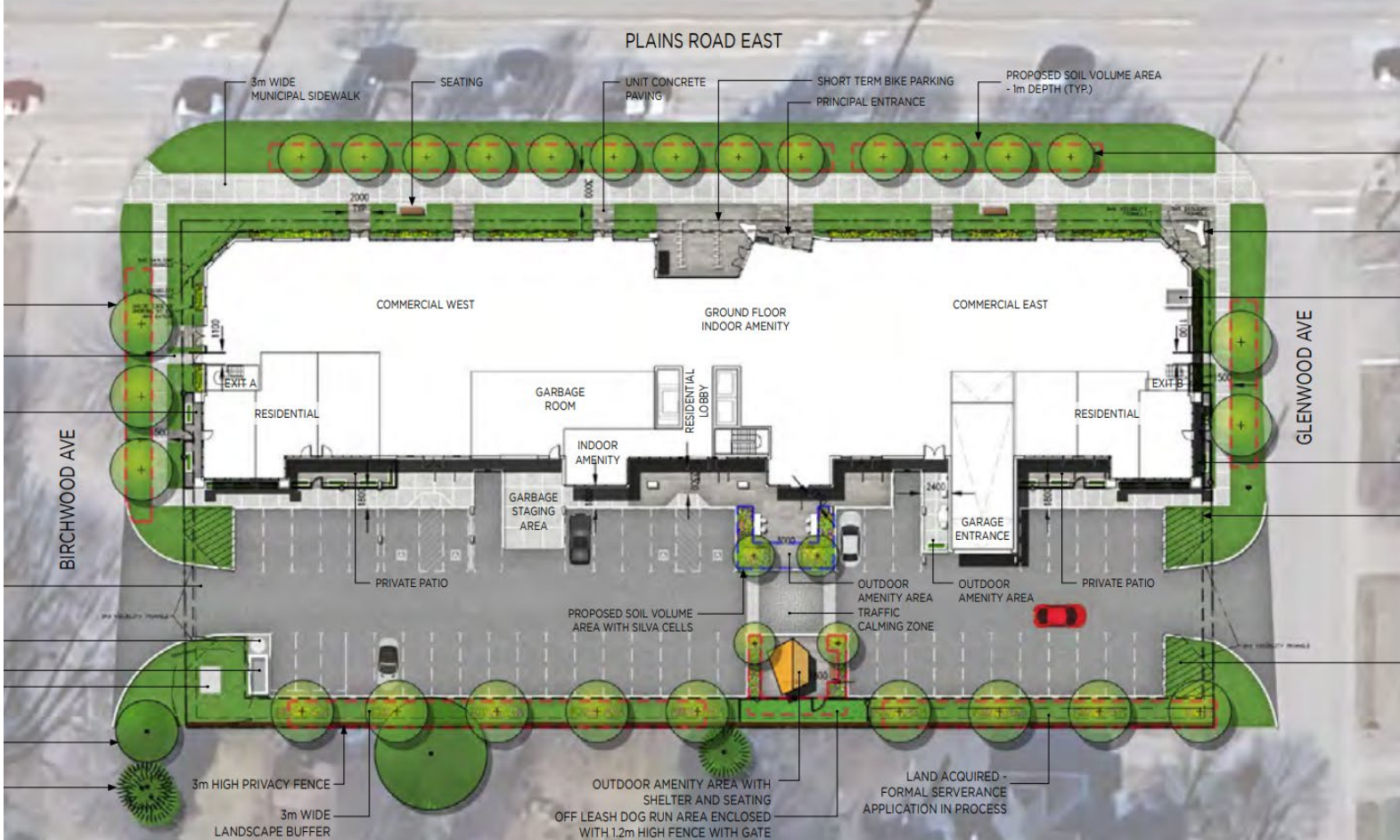
Steps-forward, not back



Summary

- Does not meet the publicly available 45-degree angular plane test
 - Secret angular plane test is not available
- Does not conform to guidance on setbacks and step-backs
- Does not conform to expert advice on height, massing, setbacks and step-backs
- Is inconsistent with height, massing, setbacks and step-backs for reasonably comparable approved proposals in the MTSA.
- Proposed mitigation measures are shown to be ineffective and inadequate.

Egress and Ingress Points Aka Driveways



Requirements

Part III, Section 2.5.2 a) (xiii): proposals for non-ground oriented housing intensification shall be permitted only at the periphery of existing residential neighbourhoods on properties abutting, and having direct vehicular access to, major arterial, minor arterial or multi-purpose arterial roads and only provided that the built form, scale and profile of development is well integrated with the existing neighbourhood so that a transition between existing and proposed residential buildings is provided.

Requirements

Part III, Section 2.5.2 a) (iii): the capacity of the municipal transportation system can accommodate any increased traffic flows, and the orientation of ingress and egress and potential increased traffic volumes to multi-purpose, minor and major arterial roads and collector streets rather than local residential streets.

Staff comment: The City's Transportation Planning staff have reviewed the application as well as the associated materials submitted and have indicated support of the application and the proposed land-use and density as the transportation network can accommodate the increase without significant negative impact. Transportation planning staff have also indicated that the proposed driveway accesses on Glenwood Avenue and Birchwood Avenue are sufficient to meet the needs of the proposed development.

Requirements

- iii) the capacity of the municipal transportation system can accommodate any increased traffic flows, and the orientation of ingress and egress and potential increased traffic volumes to multi-purpose, minor and major arterial roads and collector streets rather than local residential streets;*

Transportation Planning staff have advised that the proposed development is expected to generate approximately 86 two-way (27 inbound and 59 outbound) trips during the weekday a.m. peak hour and 100 two-way (60 inbound and 40 outbound) trips during the weekday p.m. peak hour. Staff have no concerns with the traffic that will be generated by the proposed development and agree with the conclusions of the submitted traffic impact study that the transportation network will not be adversely impacted. This criterion has been met.

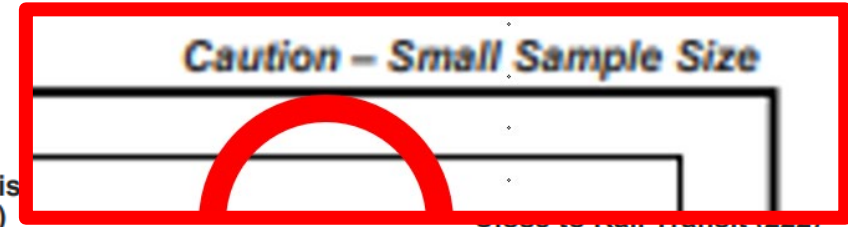
Evidence – forecasting trip generation

3.4.2 Trip Generation

The *Institute of Transportation Engineers (ITE) Trip Generation Manual (11th edition)* was used to forecast trip generation for this site. Land use code (LUC) 222 – Multifamily Housing was used for the estimation of residential trips into and out of the proposed development. For the commercial outlets included in the development, LUC 822 – Strip Retail Plaza was used to estimate trip generation. These two LUCs most accurately describe the properties and intended uses of the site, the former being residential units with greater than 10 floors of living space and the latter referring to commercial establishments with a total gross floor area (GFA) of less than 40,000 ft² (3,716 m²). The data sheets containing the parameters used in the calculation of trip generation can be found in Appendix C.

¹ ITE recommends using average rates for land uses with fewer than 20 data points and fitted curve values for land uses with 20 or more data points. For this analysis, The fitted curve values were only applied for the PM peak hour for Land Use Code 822 which has more than 20 data points available in the ITE Trip Gen Manual.

Appendix C - Multifamily Housing (High Rise) Close to Rail Transit A Small Sample



Multifamily Housing (High-Rise) Close to Rail Transit (222)

Vehicle Trip Ends vs: Residents
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 1
Avg. Num. of Residents: 250
Directional Distribution: 50% entering, 50% exiting

Multifamily Housing (High-Rise) Close to Rail Transit (222)

Vehicle Trip Ends vs: Residents
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban
Number of Studies: 1
Avg. Num. of Residents: 250
Directional Distribution: 5% entering, 95% exiting

Vehicle Trip Ends vs: Residents
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban
Number of Studies: 1
Avg. Num. of Residents: 250
Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
2.10	2.10 - 2.10	***

Vehicle Trip Generation per Resident

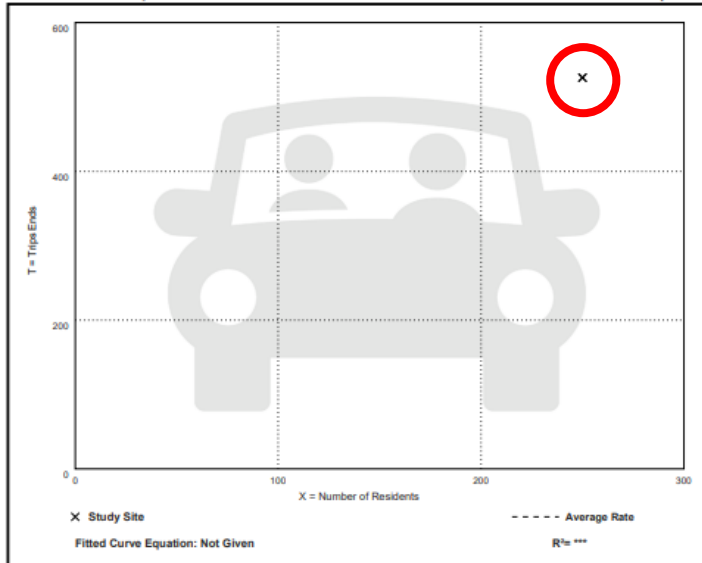
Average Rate	Range of Rates	Standard Deviation
0.23	0.23 - 0.23	***

Vehicle Trip Generation per Resident

Average Rate	Range of Rates	Standard Deviation
0.20	0.20 - 0.20	***

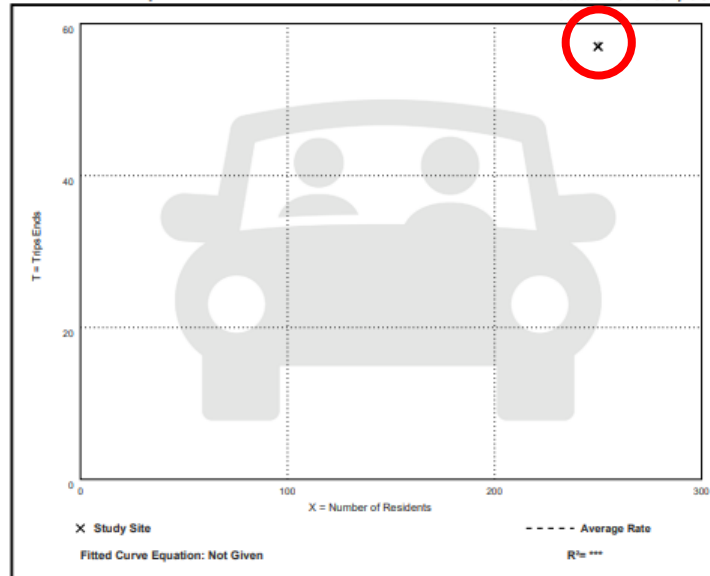
Data Plot and Equation

Caution - Small Sample Size



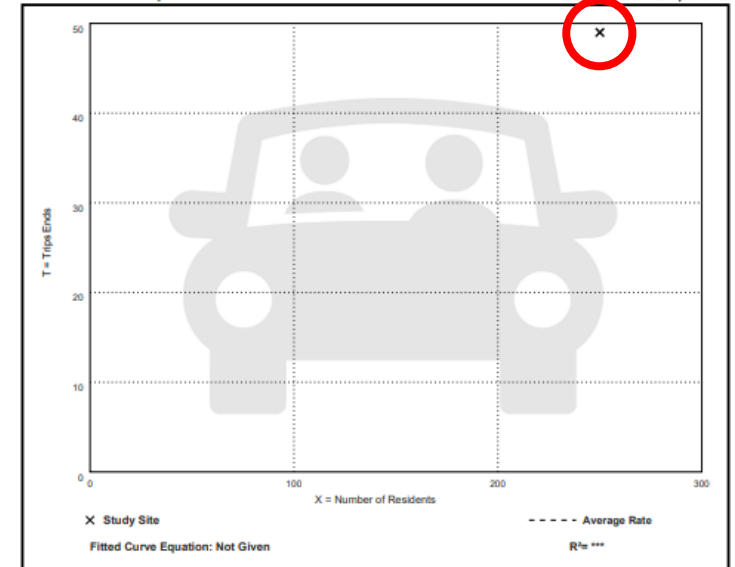
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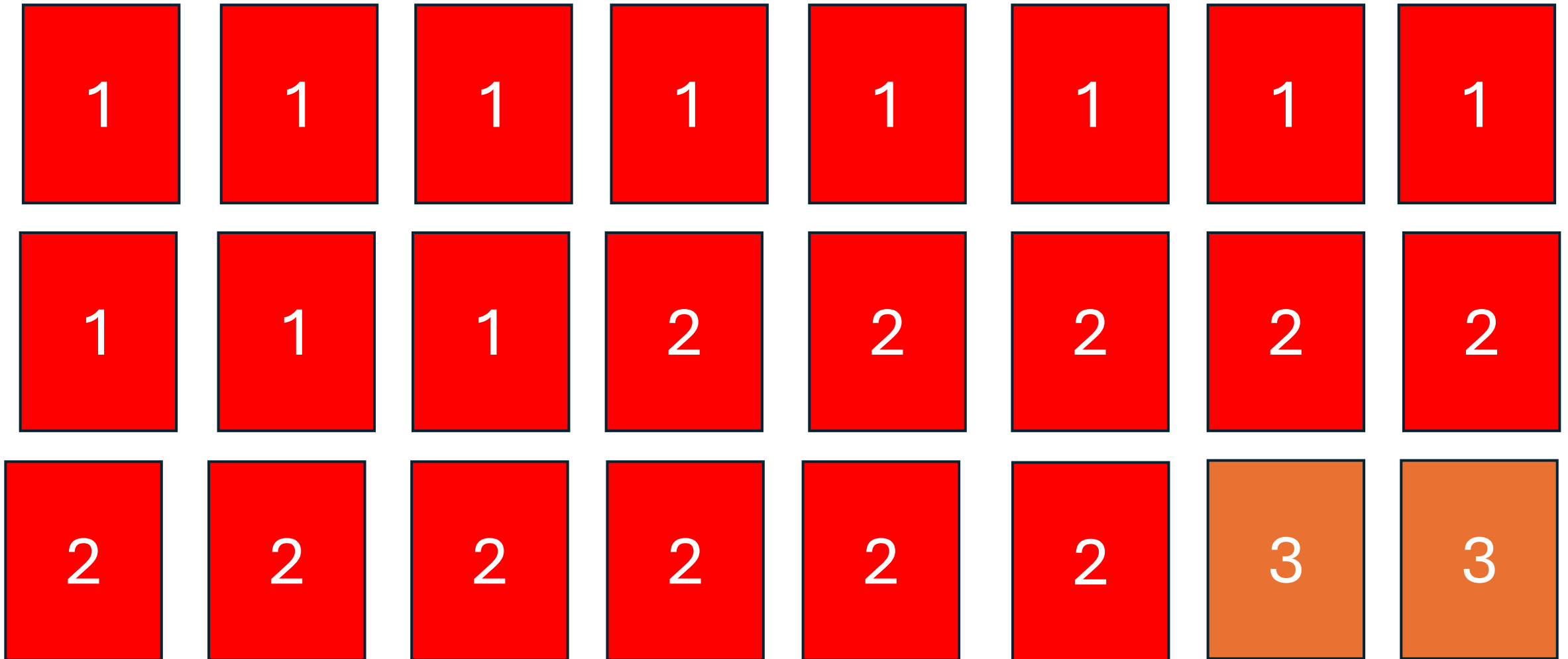


Data Plot and Equation

Caution - Small Sample Size



Summary of Appendix C – Multifamily Housing (High Rise) Close to Rail Transit



The sites were surveyed in the 1980s, the 2000s, and the 2010s in California, District of Columbia, Maryland, New Jersey, New York, Ontario (CAN), Oregon, Pennsylvania, and Virginia.

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- [Record of Site Condition](#) prepared by Province of Ontario dated February 21, 2018.
- [Notification of Intent to Use Non-Potable Groundwater Site Condition Standards](#) prepared by Halton Region dated May 6, 2025.
- [Waste Management Plan](#) prepared by Kozlowski Architect dated May 21, 2025.
- [Waste Management Report](#) prepared by Kozlowski Architect dated January 27, 2025
- [Reliance Letter for Functional Servicing Report](#) prepared by Trafalgar Engineering Ltd. dated May 12, 2025.
- [Urban Design Brief](#) prepared by MHBC Planning Ltd. dated May 2025.
- [Electrical Plans](#) prepared by EJS Engineering dated May 6, 2025.
- [Revised Land Use Compatibility Study](#) prepared by RWDI Air Inc. dated July 31, 2025.
- [Tenant Relocation and Assistance Plan](#) prepared by Miura Development Group Inc. dated May 23, 2025.
- [Tree 305 Implications](#) prepared by Miura Development Group dated May 9, 2025.
- [Revised Conceptual Site Plan and Architectural Plans](#) prepared by Kozlowski Architect dated July 28, 2025.
- [Revised Angular Plane](#) prepared by Kozlowski Architect dated August 14, 2025

The supporting documents have been uploaded on the City's website for the subject application which can be found on the following link www.burlington.ca/100plainsroad

Supporting Documents

Description

Air Quality Assessment

[Download Air Quality Assessment - 11-22-2024]

Application Form

[Download Application Form - 05-16-2025]

Arborist Report

[Download Arborist Report - 05-09-2025]

City of Burlington and Halton Region Letter of Reliance

[Download City of Burlington and Halton Region Letter of Reliance - 05-12-2025]

Conceptual Landscape Plan

[Download Conceptual Landscape Plan - 05-09-2025]

Conceptual Site Plan and Architectural Plans

[Download Conceptual Site Plan and Architectural Plans - 05-09-2025]

Electrical Plans

[Download Electrical Plans - 05-06-2025]

Elevation Renderings

[Download Elevation Renderings - 06-11-2025]

ESA Validity Letter

[Download ESA Validity Letter - 08-08-2019]

ESSQ

[Download ESSQ - 01-27-2025]

FSR and SWM Report

[Download FSR and SWM Report - 05-12-2025]

Geotechnical Study Hydrogeological Assessment

[Download Geotechnical Study Hydrogeological Assessment - 06-06-2025]

Grading Plan

[Download Grading Plan - 05-12-2025]

Housing Impact Statement

[Download Housing Impact Statement - 01-05-2025]

Land Use Compatibility Study

[Download Land Use Compatibility Study - 05-23-2025]

Letter of Reliance

[Download Letter of Reliance - 05-09-2025]

Noise and Vibration Study

[Download Noise-and-Vibration-Study-05-08-2025]

Non Potable Notification

[Download Non Potable Notification - 05-06-2025]

Pedestrian Level Wind Study

[Download Pedestrian Level Wind Study - 05-08-2025]

Phase One ESA

[Download Phase One ESA]

Phase One ESA - 92 Plains Rd E

[Download Phase One ESA - 92 Plains Rd E]

Phase Two ESA

[Download Phase Two ESA]

Phase Two ESA - 92 Plains Rd E

[Download Phase Two ESA - 92 Plains Rd E]

Planning Justification Report

[Download Planning Justification Report - 01-05-2025]

Planning Justification Report

[Download Planning Justification Report - 01-05-2025]

Record of Site Condition

[Download Record of Site Condition - 02-21-2018]

Revised Land Use Compatibility Study

[Download REVISED-Land-Use-Compatibility-Study---7-31-2025]

Servicing Plan

[Download Servicing Plan - 05-12-2025]

Shadow Study

[Download Shadow Study - 11-18-2024]

Survey

[Download Survey - 01-18-2024]

TIS TDM Paving Justification

[Download TIS TDM Paving Justification - 05-23-2025]

Tree Inventory and Preservation Plan

[Download Tree Inventory and Preservation Plan - 05-09-2025]

Urban Design Brief

[Download Urban Design Brief - 01-05-2025]

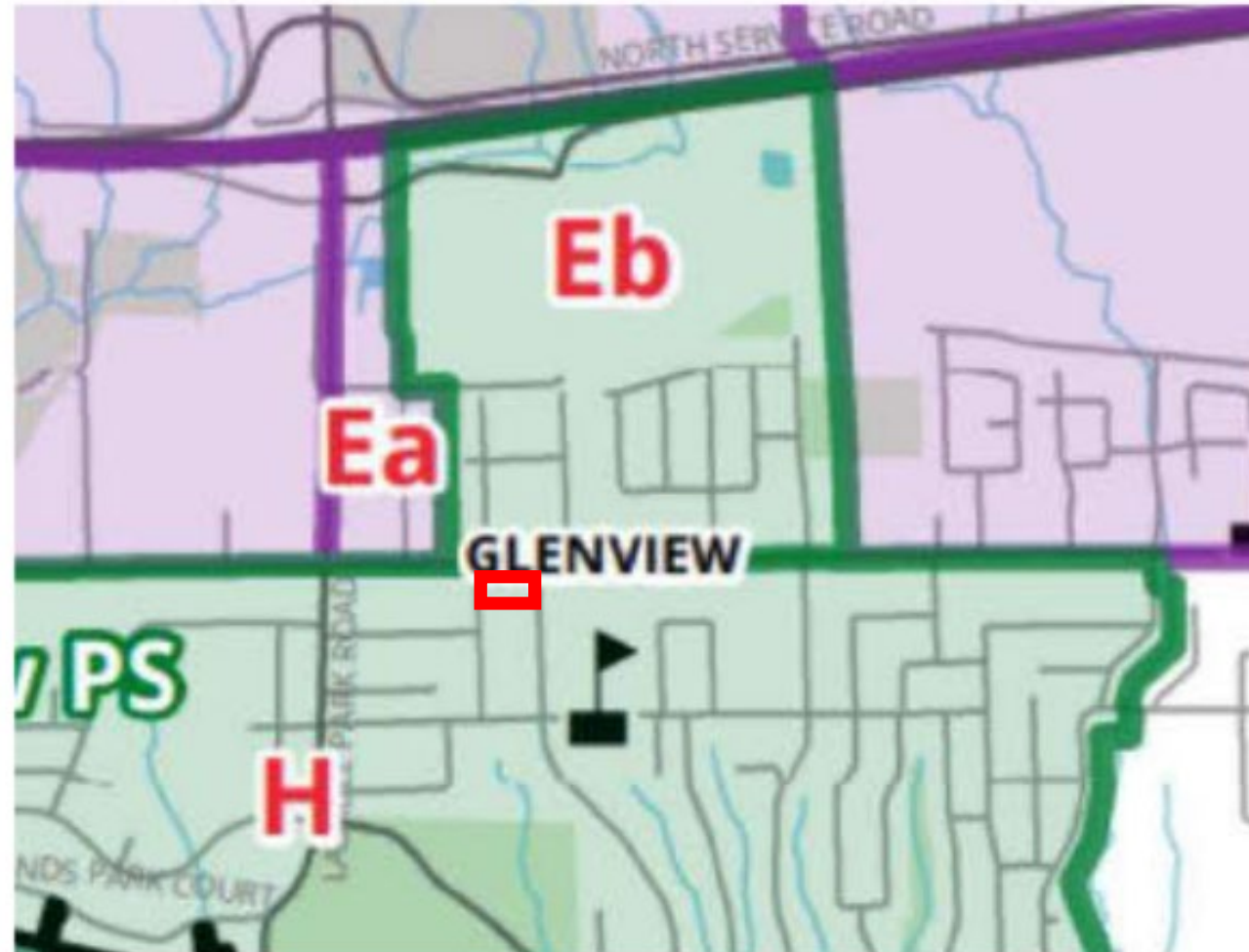
Waste Management Plan

[Download Waste Management Plan - 05-21-2025]

Waste Management Report

[Download Waste Management Report - 01-27-2025]

Pedestrians: MTSA vs Glenview Catchment



Summary

- Does not meet OP requirements for ingress and egress points NOT to be on residential streets
- Does not meet OP and PPS requirement for integration with existing neighborhood
- Data for forecasting future trips is insufficient and inadequate
- Future pedestrian travel to local school not considered, putting children at risk